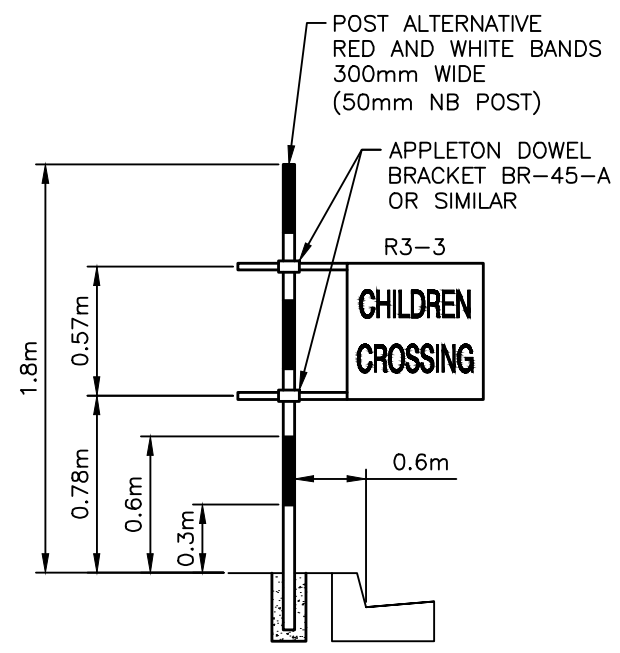


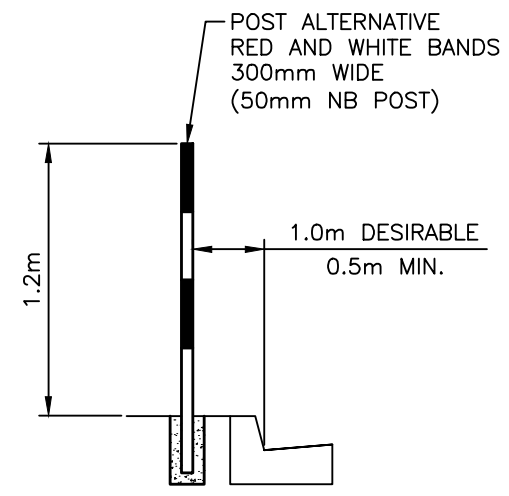
NOTE:
CONSULT WITH MANAGER—ROAD SAFETY,
QUEENSLAND TRANSPORT WHEN PROPOSING
CHANGES TO EXISTING SUPERVISED CROSSINGS
OR INSTALLING NEW SUPERVISED CROSSINGS.

NOTES:

- ISLAND KERBS AND NOSE TO BE PAINTED WHITE. MINIMUM WIDTH OF ISLAND SHOULD BE 2.4m OR GREATER.
- LENGTH OF PAINTED MEDIAN SHOULD BE INCREASED OR OTHER DELINEATION DEVICES CONSIDERED IF VISIBILITY TO THE ISLAND IS REDUCED BY VERTICAL OR HORIZONTAL ALIGNMENT. RAISED RETROREFLECTIVE PAVEMENT MARKERS ARE PROVIDED AT 5m SPACINGS.
- WHERE ISOLATED REFUGES ARE USED, PEDESTRIANS OR CHILDREN WARNING SIGNS (W6-1 OR W6-3 - MINIMUM SIZE B - SEE CLAUSE 10.2.2 & 10.2.4 M.U.T.C.D.), AS APPROPRIATE, ARE ERECTED TOGETHER WITH SUPPLEMENTRY PLATE REFUGE ISLAND (W8-25 - SEE CLAUSE 10.2.6 M.U.T.C.D.) IN ADVANCE OF THE REFUGE.
- KERB RAMPS SHOULD BE INSTALLED WITH CONCRETE PADS ON EACH SIDE OF RAMP (AS INDICATED (i)) IF NO CONCRETE FOOTPATH, INSTALL CONCRETE APRON BEHIND KERB RAMP.
- WHEN INSTALLED AT INTERSECTIONS, THE LENGTH OF THE INNERMOST ISLAND MAY BE REDUCED TO ACCOMMODATE TURNING TRAFFIC. A SUGGESTED MINIMUM LENGTH IS 1.8m.
- A HAZARD MARKER (D4-3(R)) MAY BE USED UNDER THE KEEP LEFT (R2-3(L)) SIGN. MOUNTING HEIGHTS NEED TO BE SELECTED SO AS TO AVOID OBSCURING VISIBILITY OF CHILD PEDESTRIANS.
- A LINE (APPROXIMATELY 100mm WIDE AND PAINTED YELLOW) TO BE PAINTED ON THE FOOTPATH - 1m BEHIND THE FACE OF THE KERB (THIS MAY BE REDUCED TO 0.5m MIN. WHERE FOOTPATH WIDTH AND VISIBILITY ARE LIMITED) - TO INDICATE THE POSITION WHERE PEDESTRIANS SHOULD WAIT UNTIL DIRECTED TO CROSS THE CARRIAGEWAY, OR IF UNSUPERVISED A SUITABLE GAP IN TRAFFIC OCCURS IN WHICH TO SAFELY CROSS THE TRAFFIC. THIS LINE EXTENDS THE WIDTH OF THE SEALED APRON CONNECTING THE FOOTPATH AND KERB OR A DISTANCE OF 3-6m I.E. BETWEEN THE CROSSING POSTS (WITHOUT FLAGS).
- WHERE THERE IS SIGNIFICANT NIGHT TIME USE, STREET LIGHTING SHOULD BE PROVIDED (SEE CLAUSE 14 M.U.T.C.D.) PRIOR TO INSTALLATION OF PEDESTRIAN CROSSING.
- IN 'CENTRAL TRAFFIC AREAS' THE APPROACH NO STANDING ZONE MAY BE REDUCED TO 9m & THE DEPART NO STANDING ZONE REDUCED TO 6m.
- ALL CONCRETE TO BE GRADE N25 AND BROOM FINISHED FOR SLIP RESISTANCE REQUIREMENTS.
- FOR DESIGN NOTES, CONSTRUCTION NOTES AND LEGEND REFER TO UMS 901.
- ALL 'W6-3' SIGNS ARE TO BE FITTED WITH FLUORO ORNAGE TARGET BOARDS.



POST AND FLAG



POST (B)

A	ORIGINAL ISSUE	Jan '11	Feb '11	Feb '11
ISSUE	AMENDMENT	DRAWN DATE	CHK'D DATE	APPR'D DATE

DESIGN AUTHORISED FOR ISSUE			
P COTTON SIGNATURE ON ORIGINAL DATED 06/09/06			
MANAGER CITY ASSETS, R.P.E.Q. 2 5 4 6			
DESIGN APPROVED			
V NASH SIGNATURE ON ORIGINAL			
SENIOR PROGRAM OFFICER - ROAD NETWORK			
DESIGN	STANDARDS WORKING GROUP	DATE	Jan '11
DRAWN	CITY DESIGN	DATE	Jan '11
CHECKED	D Maher	DATE	Jan '11
DRAWING FILENAME	UMS 995		
ASSOCIATED PLANS			



BRISBANE CITY COUNCIL STANDARD DRAWINGS

ROAD NETWORK GUIDELINES
CHILDREN'S CROSSING WITH PEDESTRIAN REFUGE
SUPERVISED

SCALE: NOT TO SCALE
DWG No. **UMS 995**
ORIGINAL SIZE: A3 REVISION: A