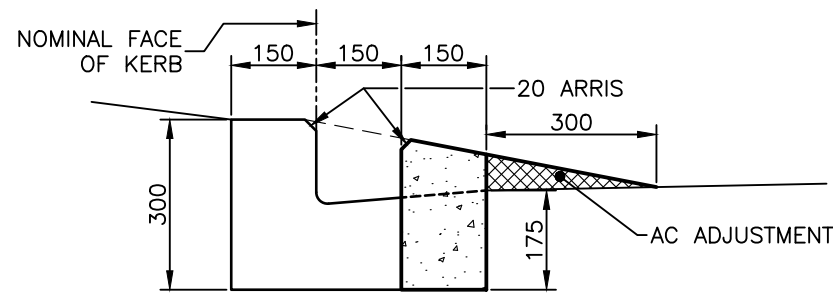
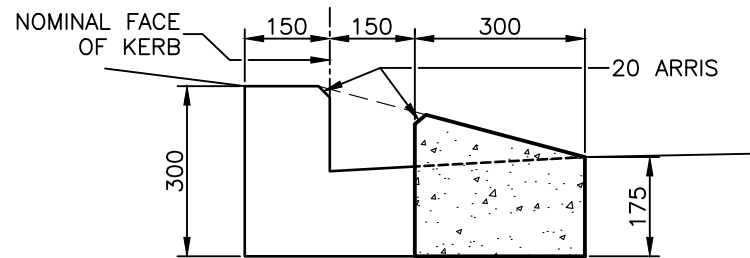


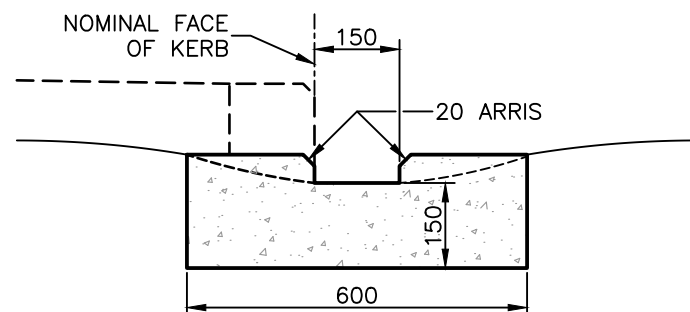
**STANDARD TYPE 'E'
KERB & CHANNEL**



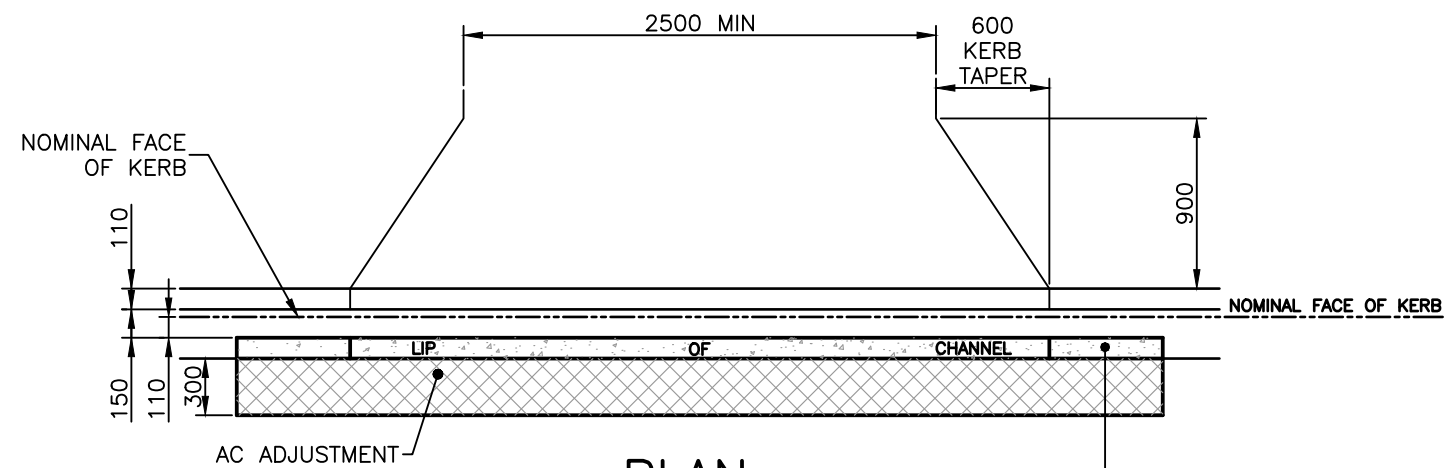
**VERTICAL FACE TYPE 'E'
KERB & CHANNEL
(300 CHANNEL)**



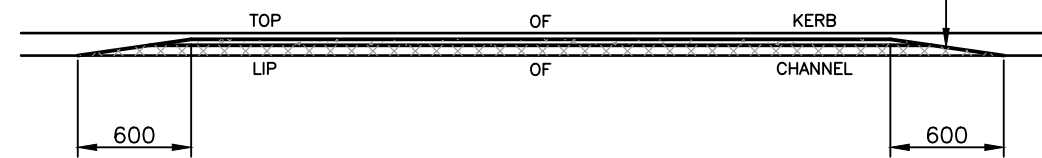
**VERTICAL FACE TYPE 'E'
KERB & CHANNEL
(450 CHANNEL)**



INVERT MODIFICATION



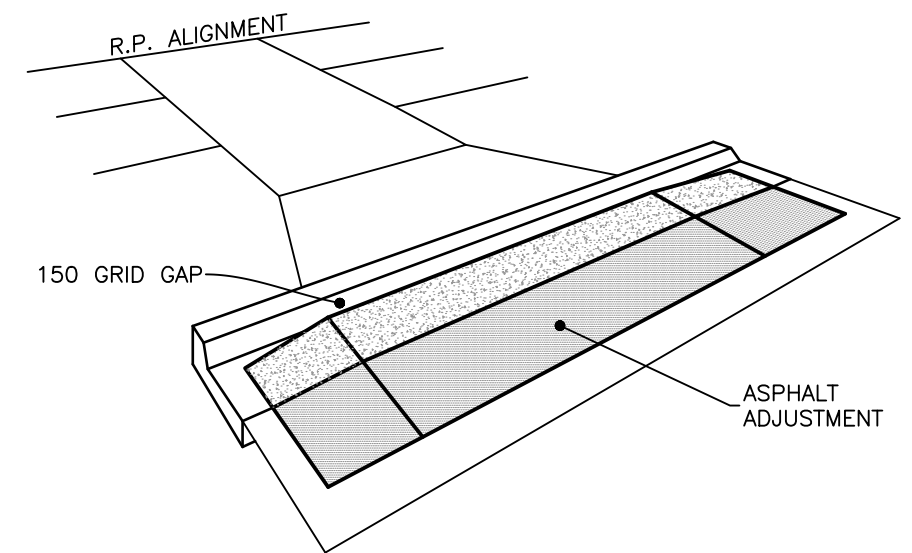
**PLAN
FOR STANDARD TYPE 'E' K&C**



**ELEVATION
FOR STANDARD TYPE 'E' K&C**

NOTES:

1. THIS TREATMENT IS NOT APPROVED FOR NEW SUBDIVISIONS.
2. GRID CROSSINGS SHALL NOT BE USED WHERE A STANDARD VEHICLE INVERT CROSSING WILL SUFFICE, AS PER UMS 223.
3. FOR LOCAL ACCESS ROADS, THE METHOD OF MODIFYING THE CHANNEL AS SHOWN MAY BE ADOPTED IN LIEU OF FULL RECONSTRUCTION.
4. GRID CROSSINGS SHALL NOT BE USED ON NEIGHBOURHOOD ACCESS ROADS AND ABOVE WITHOUT PRIOR APPROVAL OF THE DISTRICT MANAGER, LOCAL ASSET SERVICES, IN WHICH CASE FULL RECONSTRUCTION OF THE KERB, CHANNEL AND INVERT WILL BE REQUIRED.
5. GRID GAP TO BE 150 WIDE.
6. ALL CONCRETE TO BE GRADE N32.
7. ALL CONCRETE TO BE STEEL TROWEL FINISHED.
8. AFTER SECTIONS OF THE CONCRETE ARE REMOVED, THE REMAINING SURFACE SHALL BE SCABBLED TO A TOLERANCE OF $\pm 10\text{mm}$ AND CLEANED PRIOR TO PLACING NEW CONCRETE.
9. DIMENSIONS IN MILLIMETRES (U.N.O.).



**GRID CROSSING
PERSPECTIVE VIEW**

ISSUE	AMENDMENT	DRAWN DATE	CHK'D DATE	APPR'D DATE
C	Notes 5 & 6 Mod., Perspective View Dim Mod.	DL 10/10	IC 12/10	PC 01/11
B	GRID LENGTH EXTENDED, PERSPECTIVE VIEW AND NOTE 5 ADDED	DL 10/02	CD 02/03	BH 02/03
A	ORIGINAL ISSUE	April '01	May '01	June '01

DESIGN AUTHORIZED FOR ISSUE B. BALL SIGNATURE ON ORIGINAL DATED 29/6/01	DESIGN	STD DWG GROUP	DATE	April '01
MANAGER ASSET SUPPORT - R.P.E.Q. 3 & 5 2	DRAWN	CITY DESIGN	DATE	April '01
DESIGN APPROVED B. HANSEN SIGNATURE ON ORIGINAL DATED 27/6/01	CHECKED	M. STEER	DATE	May '01
PRINCIPAL ASSET OFFICER ROADS & DRAINAGE	DRAWING FILENAME	UMS 215		
	ASSOCIATED PLANS	SUPERSEDES WS 11-5		



BRISBANE CITY COUNCIL STANDARD DRAWING

GRID CROSSING AND INVERT MODIFICATION

SCALE	NOT TO SCALE
DWG No.	UMS 215
ORIGINAL SIZE	A3
REVISION	C