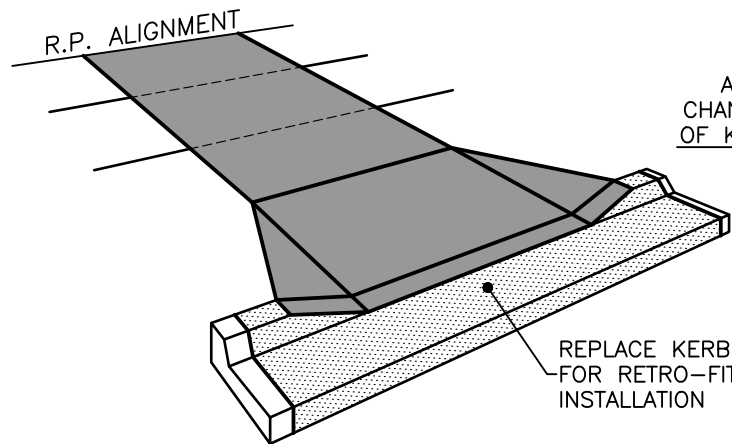
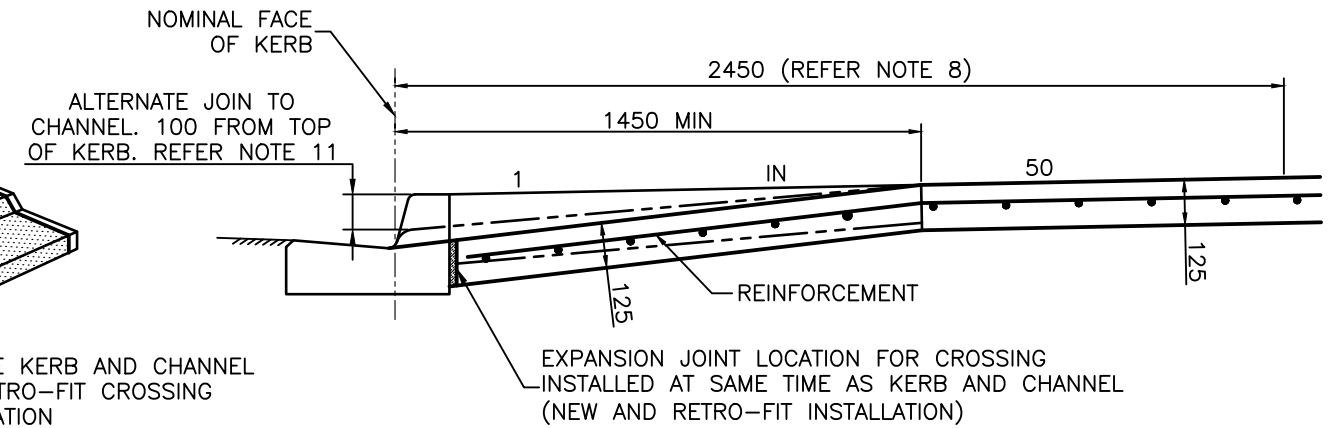


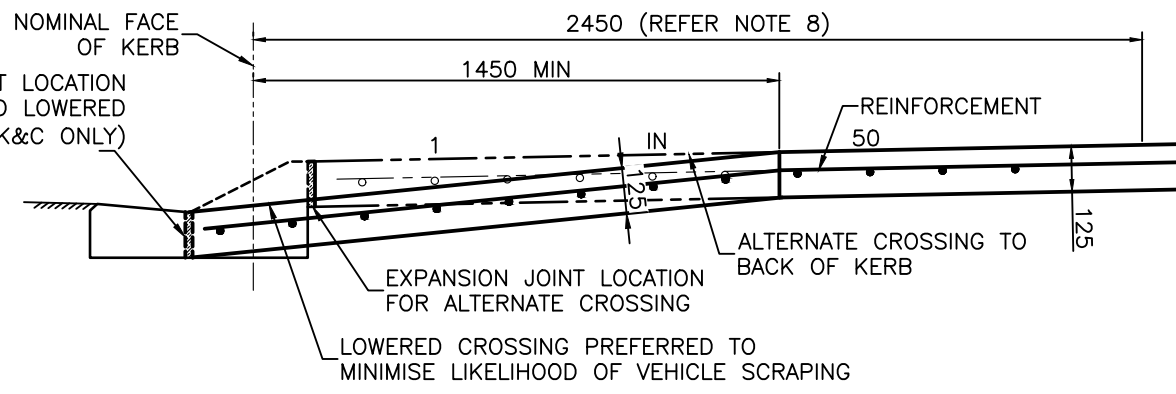
**SECTION A-A  
(FOR TYPE 'E' K&C)**



**PERSPECTIVE VIEW**



**SECTION (WITH TYPE 'E' K&C)**

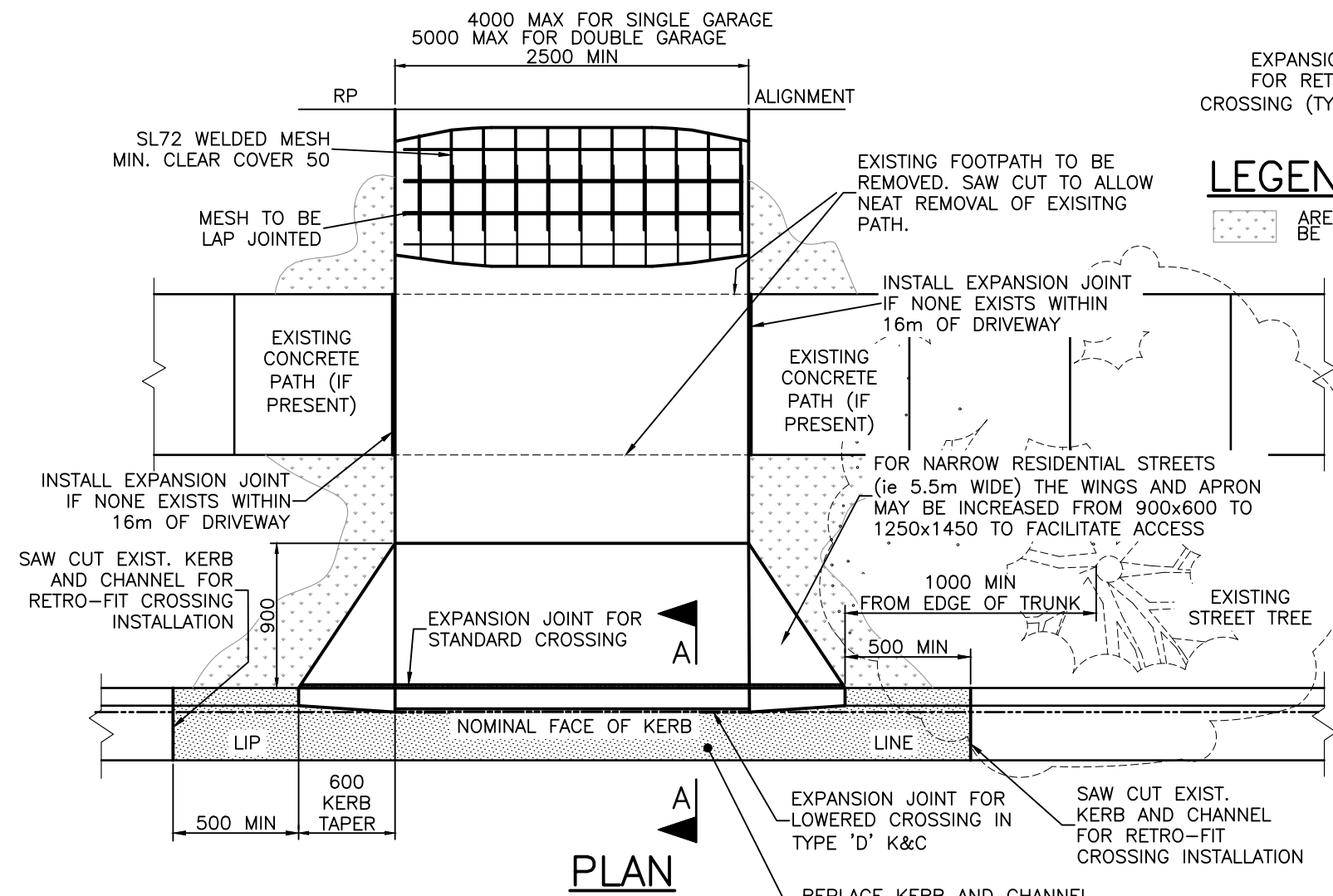


**SECTION (WITH TYPE 'D' K&C)**

**LEGEND:**  
 AREA TO BE TURFED

**NOTES:**

1. THE SPECIFIED PAVEMENT STANDARD DOES NOT APPLY TO POOR SUBGRADE. REFER SUPPLEMENTARY NOTES FOR DETAIL (REFER UMS.H FOR REQUIREMENTS).
2. THIS CROSSING IS NOT FOR COMMERCIAL VEHICLES.
3. ALL CONCRETE TO BE GRADE N32.
4. ALL CONCRETE TO BE BROOM FINISHED. FOR SLIP RESISTANCE REQUIREMENTS REFER SPECIFICATION S150 ROADWORKS.
5. EXPANSION JOINTS TO BE FULL DEPTH 10 THICK CLOSED CELL CROSS LINKED POLYETHYLENE FOAM (85-150kg/m<sup>3</sup>). SEAL SURFACE OF JOINT WITH A SUITABLE POLYURETHANE SEALANT.
6. THE THICKNESS OF DECORATIVE SURFACING WHERE APPROVED IS ADDITIONAL TO THE THICKNESS SHOWN.
7. PERMITS RELATING TO ROADS AND DRAINAGE MUST BE OBTAINED FROM COUNCIL (EITHER FROM REGIONAL BUSINESS CENTRES OR [www.brisbane.qld.gov.au](http://www.brisbane.qld.gov.au)) TO SEEK APPROVAL OF LOCATION AND LEVELS PRIOR TO ANY EXCAVATION.
8. A 1:50 SLOPE UPWARDS FROM THE TOP OF KERB FOR A MIN DISTANCE OF 2450mm IS NECESSARY TO PROTECT PROPERTIES FROM STORMWATER OVERFLOW FROM THE ROAD PAVEMENT. THIS CONSTRAINT MAY BE VARIED WITH THE APPROVAL FROM THE MANAGER, CITY ASSETS BRANCH OR HIS DELEGATE.
9. WHEN CROSSING IS RETRO-FITTED TO EXISTING KERB AND CHANNEL, THE EXISTING KERB AND CHANNEL IS TO BE REMOVED USING A SAW CUT AND NEW KERB AND CHANNEL CONSTRUCTED AS PART OF NEW CROSSING. REFER UMS 211 FOR STANDARD KERB PROFILES.
10. FOR RETRO-FITTED CROSSINGS EXISTING ROAD PAVEMENT TO BE SAW CUT TO ALLOW CLEAN REMOVAL OF EXISTING KERB AND CHANNEL. REINSTATE ROAD PAVEMENT AFTER CONSTRUCTION OF NEW CROSSING.
11. INVERT CHANNEL LIP MAY BE PROVIDED ON BOTH TYPE 'D' AND TYPE 'E' KERB AND CHANNEL TYPES TO PREVENT WATER FLOW INGRESS ON FLAT OR NEGATIVE VERGE CROSSFALLS. LIP TO BE 100mm FROM TOP OF KERB.
12. DIMENSIONS IN MILLIMETRES (U.N.O.).



**PLAN**

ISSUE	AMENDMENT	DRAWN DATE	CHK'D DATE	APPR'D DATE
F	Footpath Thru Crossing Requirements Mod.	DJL 01/10	IC 06/10	PC 06/10
E	Channel Lip Option & Street Tree Req Added, Type 'D' CKC Options Mod, Notes 1 & 3 Mod, Note 11 Added, Title Block Mod.	DJL 02/09	IC 09/09	PC 9/09
D	Retro-fit Requirements Mod to Plan, Sections and Perspective, Note 9 Mod, Note 10 Added	DJL 09/05	BH 13/03/06	PC 21/03/06
C	Expansion Joint Added at Joint to Existing Path, Reinforcing Mesh Details Updated.	DJL 12/04	BH 14/02/05	PC 03/03/05
B	INVERT CROSS MOD, LIP REMOVED. NOTE 9 ADDED	DL 10/02	CD 02/03	BH 02/03
A	ORIGINAL ISSUE	April '01	May '01	June '01

DESIGN AUTHORISED FOR ISSUE  
 B. BALL SIGNATURE ON ORIGINAL DATED 29/6/01

MANAGER ASSET SUPPORT - R.P.E.Q: 3 8 5 2  
 DESIGN APPROVED  
 B. HANSEN SIGNATURE ON ORIGINAL DATED 27/6/01

PRINCIPAL ASSET OFFICER ROADS & DRAINAGE

DESIGN	STD DWG GROUP	DATE	April '01
DRAWN	CITY DESIGN	DATE	April '01
CHECKED	M. STEER	DATE	May '01
DRAWING FILENAME	UMS 223		
ASSOCIATED PLANS	SUPERSEDES WS 12-3		



**BRISBANE CITY COUNCIL STANDARD DRAWING**

**VEHICLE CROSSING (SLAB) FOR SINGLE DWELLING**

SCALE: NOT TO SCALE  
 DWG No. UMS 223  
 ORIGINAL SIZE: A3  
 REVISION: F