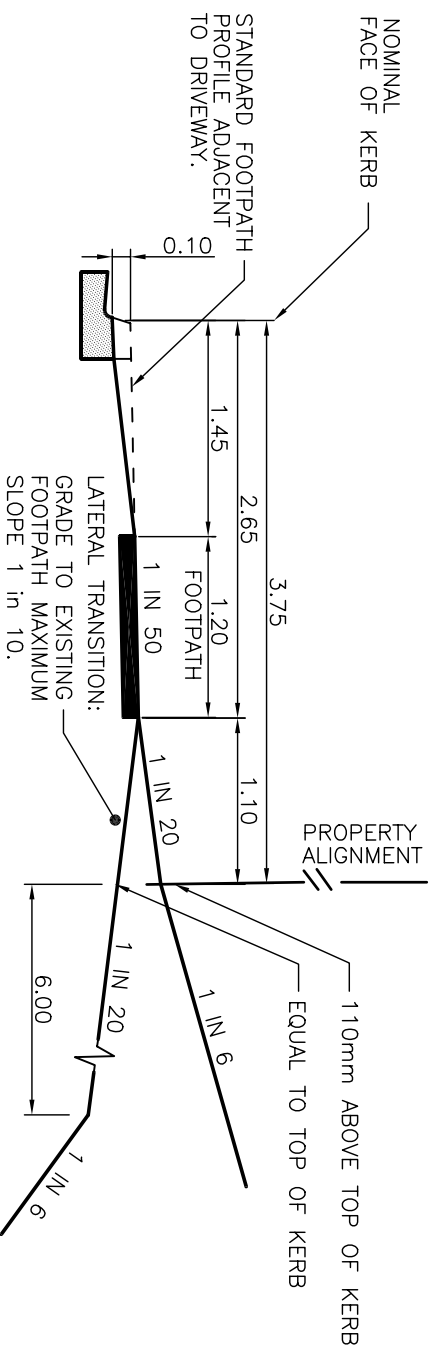
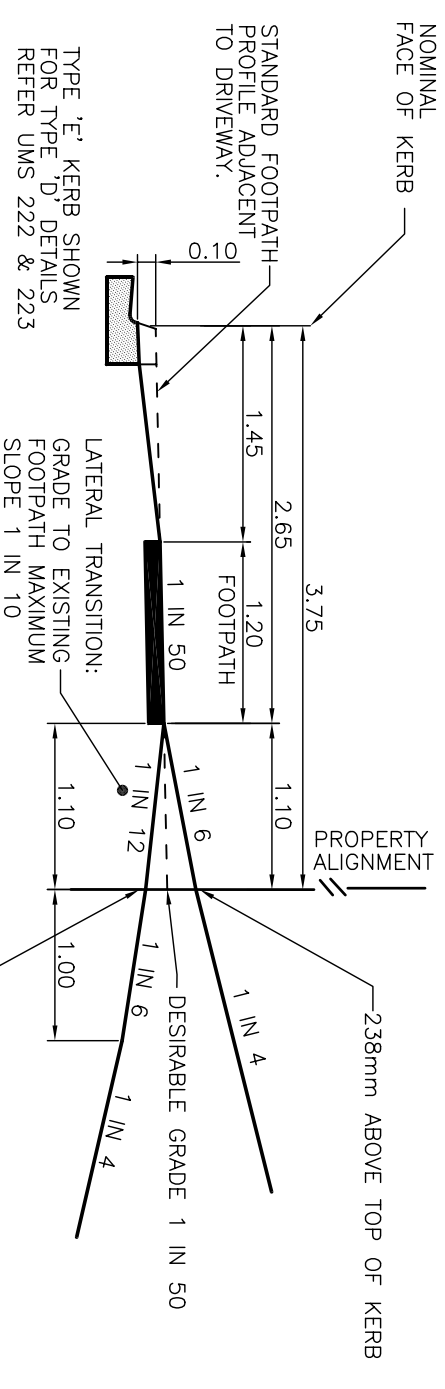


DESIRABLE GRADES



MAXIMUM GRADES

NON-RESIDENTIAL VEHICULAR ENTRANCES & RESIDENTIAL (OTHER THAN SINGLE DWELLING)



MAXIMUM GRADES

RESIDENTIAL VEHICULAR ENTRANCE (SINGLE DWELLING)

NOTES:

1. ALTERNATIVE GRADES MAY BE ADOPTED TO SUIT SPECIFIC SITUATIONS AND TYPES OF VEHICLES AS SPECIFIED IN THE "TRANSPORT, ACCESS, PARKING AND SERVICING PLANNING SCHEME POLICY" OF BRISBANE CITY PLAN.
2. ROUNDING MAY BE REQUIRED WHERE CHANGE IN PROFILES EXCEED 5%. REFER TO BRISBANE CITY PLAN. GENERALLY A ROUNDING APPROXIMATING TO THE AVERAGE SLOPE OVER 6.4m IS SUITABLE.
3. WHEN FOOTPATH WIDTH EXCEEDS 3.75m BUT IS LESS THAN 4.25m WIDE THE DESIRABLE POSITION OF STRIP FOOTPATH IS 1.1m FROM PROPERTY ALIGNMENT.
4. THE NEED FOR A FLAT APPROACH GRADE (EG. QUEUE LENGTH) FROM THE PRIVATE PROPERTY TO THE FOOTPATH IS DEPENDENT ON CARPARK CAPACITY AND TURNOVER RATES. THE HIGHER THE CAPACITY AND EXPECTED TURNOVER RATES, THE MORE CRITICAL ARE THESE REQUIREMENTS. REFER "TRANSPORT, ACCESS, PARKING AND SERVICING PLANNING SCHEME POLICY" OF BRISBANE CITY PLAN.
5. THE LAYOUTS OF DRIVEWAYS ARE DETAILED IN STANDARD DRAWINGS UMS 221, UMS 222 & UMS 223.
6. DIMENSIONS IN METRES (U.N.O.).

ISSUE				
A	ORIGINAL ISSUE	APRIL '01	MAY '01	JUNE '01
	AMENDMENT			

DESIGN AUTHORISED FOR ISSUE B. BALL SIGNATURE ON ORIGINAL DATED 29/6/01	DESIGN DRAWN	STD DWG GROUP CITY DESIGN	DATE APRIL '01
MANAGER ASSET SUPPORT - R.P.E.G. 3, 8, 5, 2 DESIGN APPROVED B. HANSEN SIGNATURE ON ORIGINAL DATED 27/6/01	CHECKED FILENAME UIMS 224	M. STEER	MAY '01
PRINCIPAL ASSET OFFICER ROADS & DRAINAGE	ASSOCIATED PLANS	SUPersedes WS 12-4	



BRISBANE CITY COUNCIL - URBAN MANAGEMENT DIVISION

STANDARD DRIVEWAY ENTRANCE GRADES (3.75m WIDE FOOTPATH)

SCALE	NOT TO SCALE
DWG No.	UMS 224
ORIGINAL SIZE	A3
REVISION	A