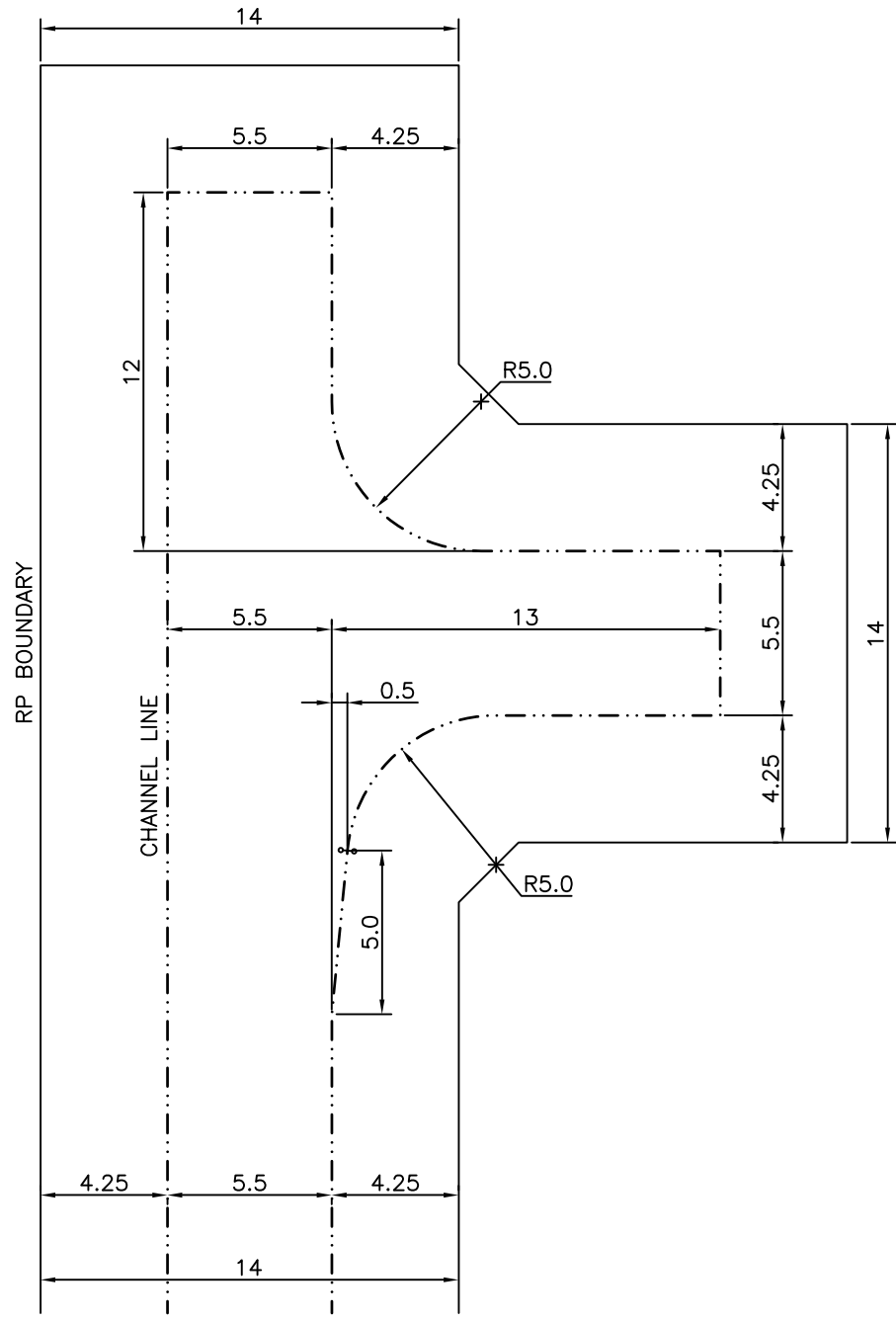


CUL-DE-SAC HEAD



OFFSET T HEAD

**NOTES:**

1. PARKING LOCATIONS IF REQUIRED MAY BE VARIED TO SUIT ALLOTMENT LAYOUT AND ROAD GEOMETRY (RATE OF 0.5 SPACES/LOT)
2. THESE TURNING AREAS ARE BASED ON THE FOLLOWING:
  - BCC SPLIT REFUSE TRUCK ABLE TO TURN WITHIN PAVED AREA AND ABLE TO TURN IN ANY DIRECTION TO ENABLE PICK UP EITHER SIDE, IN EACH ARM
  - SUBJECT TO SERVICE REQUIREMENTS THE VERGE MAY BE REDUCED TO A MINIMUM OF 3.0m AT PINCH POINTS.
3. ALL DIMENSIONS IN METRES (U.N.O.).

ISSUE	AMENDMENT	DRAWN DATE	CHK'D DATE	APPR'D DATE
A	ORIGINAL ISSUE	Jul '04	Nov '05	Mar '06

DESIGN AUTHORISED FOR ISSUE P COTTON SIGNATURE ON ORIGINAL DATED 21/03/06				DESIGN	BCC (IMB)	DATE	Jul '04
MANAGER CITY ASSETS, R.P.E.Q. 2546				DRAWN	CA (DJL)	DATE	Jul '04
DESIGN APPROVED				CHECKED	CA (GMc)	DATE	Nov '05
B HANSEN SIGNATURE ON ORIGINAL DATED 13/03/06				DRAWING FILENAME	UMS 812.dwg		
PRINCIPAL ENGINEER STRATEGIC INFRASTRUCTURE MANAGEMENT				ASSOCIATED PLANS			



**BRISBANE CITY COUNCIL - CITY POLICY & STRATEGY DIVISION**

**TYPICAL MANOEUVRING AREAS - RESIDENTIAL STREET (SHEET 2 of 2)**

SCALE 1:250	
DWG No. UMS 812	
ORIGINAL SIZE A3	REVISION A