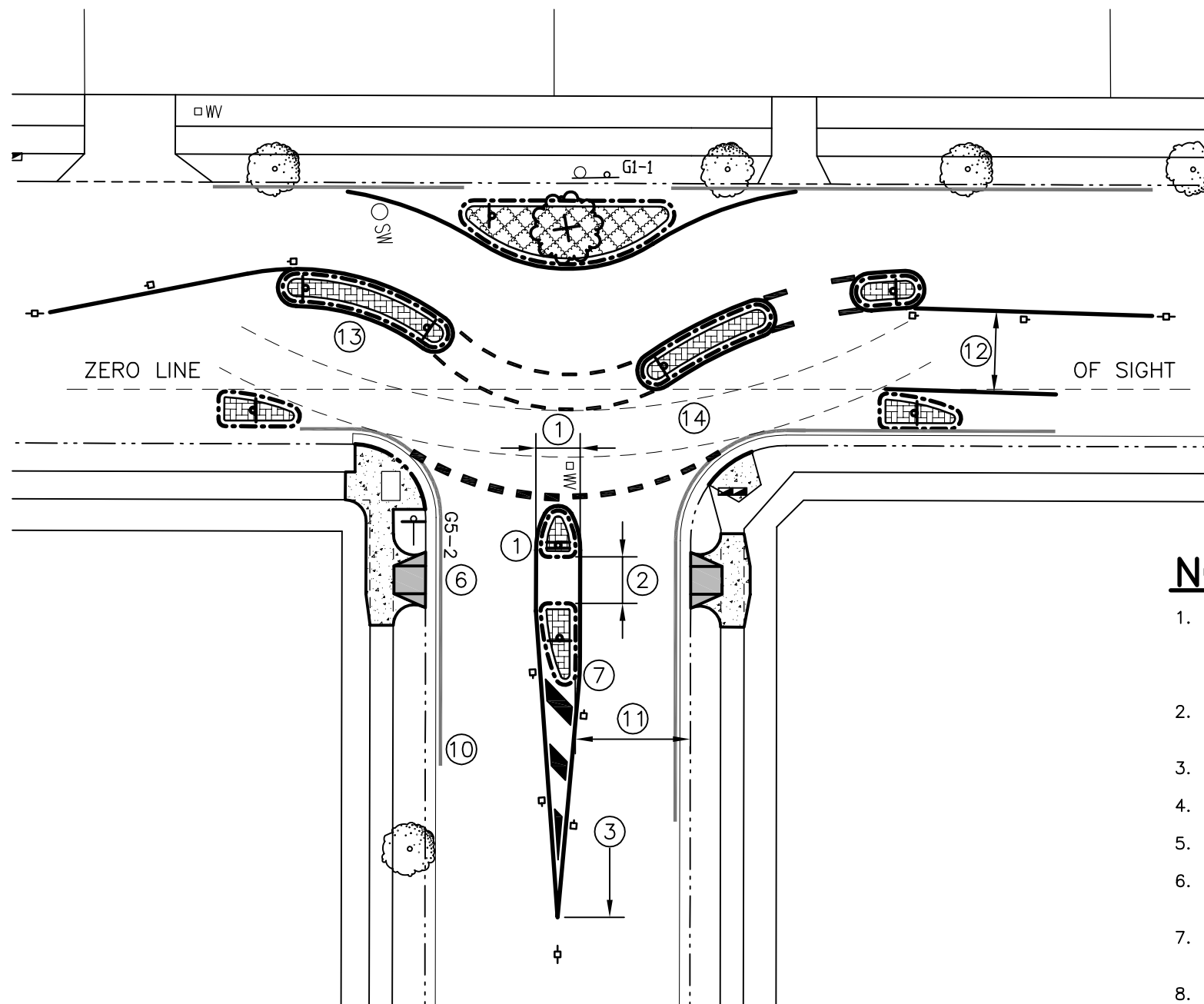


INTERSECTION TAIL LENGTH TABLE

ISLAND WIDTH (m)	SPEED (Km/h)					
	20	30	40	50	60	70
1.0	6	8	11	14	19	23
1.5	8	11	15	19	26	31
2.0	10	14	19	24	33	39
2.4	11	17	22	28	39	45
3.0	13	20	27	34	47	55

NOTE

FIGURES ONLY APPLY TO ISLANDS LOCATED CENTRALLY TO ROAD CENTRELINE



PLAN

NOTES

1. PRECAST TRAFFIC ISLANDS AS PER UMS 981 AND UMS 982 TO BE USED AT PEDESTRIAN REFUGES.
 - DESIRABLE WIDTH 2.4m. WIDTH (BETWEEN PAINTED ISLAND LINES) TO BE 2.8m MINIMUM.
 - DESIRABLE MINIMUM WIDTH 2.0m. WIDTH (BETWEEN PAINTED ISLAND LINES) TO BE 2.4m MINIMUM.
 - ABSOLUTE MINIMUM WIDTH 1.5m. WIDTH (BETWEEN PAINTED ISLAND LINES) TO BE 1.9m MINIMUM.
2. SPACING BETWEEN ISLANDS TO BE 2.0m MINIMUM. SPACING TO BE INCREASED TO 2.4m AT BICYCLE CROSSING FACILITIES, OR WHERE HOLDING RAILS REQUIRED (REFER NOTE 9).
3. FOR LENGTH OF PAINTED ISLAND TAILS, REFER TO 'TAIL LENGTH' TABLE. RRPM'S AT 5.0m SPACING.
4. BICYCLE AWARENESS PAVEMENT SYMBOLS OPTIONAL OR AS SPECIFIED (REFER UMS 861).
5. WARNING SIGNAGE W6-1/W8-25 REQUIRED AT ISOLATED REFUGES, 60-80m IN ADVANCE OF REFUGE ISLANDS.
6. DIRECTIONAL KERB RAMPS AS PER UMS 213 TO BE CONSTRUCTED, WITH CONCRETE FOOTPATH CONNECTION TO EXISTING. A MINIMUM OF 1.2m OF CONCRETE FOOTPATH IS REQUIRED BEHIND BACK OF KERB RAMP.
7. LENGTH OF REFUGE ISLAND MAY BE REDUCED TO 2.0m MINIMUM TO ALLOW FOR VEHICLE MOVEMENTS AT PROPERTY ACCESS.
8. STREET LIGHTING TO BE PROVIDED IN ACCORDANCE WITH AS 1158.
9. PEDESTRIAN HANDRAILS, 0.9m HIGH, TO BE INSTALLED WHERE REQUIRED. BICYCLE HANDRAILS, 1.2m HIGH TO BE INSTALLED AT BICYCLE CROSSING FACILITIES. (REFER UMS 924 FOR DETAILS).
10. NO STOPPING RESTRICTIONS TO BE DELINEATED BY YELLOW 'NO STOPPING' EDGE LINE. EXTENT OF RESTRICTIONS TO BE DETERMINED AS PER 'CROSSING SIGHT DISTANCE' TABLE (REFER UMS 924).
11. WIDTH FROM REFUGE ISLAND TO KERB - 4.2m DESIRABLE (3.1m MINIMUM).
12. LANE WIDTH PAST ISLAND (TO EDGE LINE) - 3.1m MINIMUM.
13. ISLAND WIDTH TO BE 1.2m MINIMUM. ISLAND WIDTH WHERE PEDESTRIAN WALK THROUGH IS REQUIRED TO BE 1.5m MINIMUM.
14. ISLANDS TO BE PLACED TO ACHIEVE MAXIMUM DEFLECTION THROUGH THE INTERSECTION. IN LOCAL TRAFFIC AREAS, GENERAL MAXIMUM DESIGN SPEED IS 40km/hr. ZERO LINE OF SIGHT IS MINIMUM CRITERIA IN ALL CASES.
15. FOR DESIGN NOTES, CONSTRUCTION NOTES AND LEGEND REFER TO UMS 901.

ISSUE	AMENDMENT	DRAWN DATE	CHK'D DATE	APPR'D DATE
B	TABLE HEADING AMENDED AND NOTE 15 ADDED	May '07	May '07	May '07
A	ORIGINAL ISSUE	May '06	June '06	Aug '06

DESIGN AUTHORISED FOR ISSUE					
P COTTON SIGNATURE ON ORIGINAL					
DATED 06/09/06					
MANAGER CITY ASSETS, R.P.E.G. 2 5 4 6					
DESIGN APPROVED					
V NASH SIGNATURE ON ORIGINAL					
SENIOR PROGRAM OFFICER - ROAD NETWORK					
DESIGN	STANDARDS WORKING GROUP	DATE	May '06		
DRAWN	CITY DESIGN	DATE	May '06		
CHECKED	D Maher	DATE	June '06		
DRAWING FILENAME	\\UMS 932				
ASSOCIATED PLANS	Supersedes UA 40 -3				



BRISBANE CITY COUNCIL - CITY POLICY & STRATEGY DIVISION

ROAD NETWORK GUIDELINES MODIFIED T JUNCTION WITHIN LOCAL TRAFFIC AREA

SCALE	NOT TO SCALE	
DWG No.	UMS 932	
ORIGINAL SIZE	A3	REVISION
		B